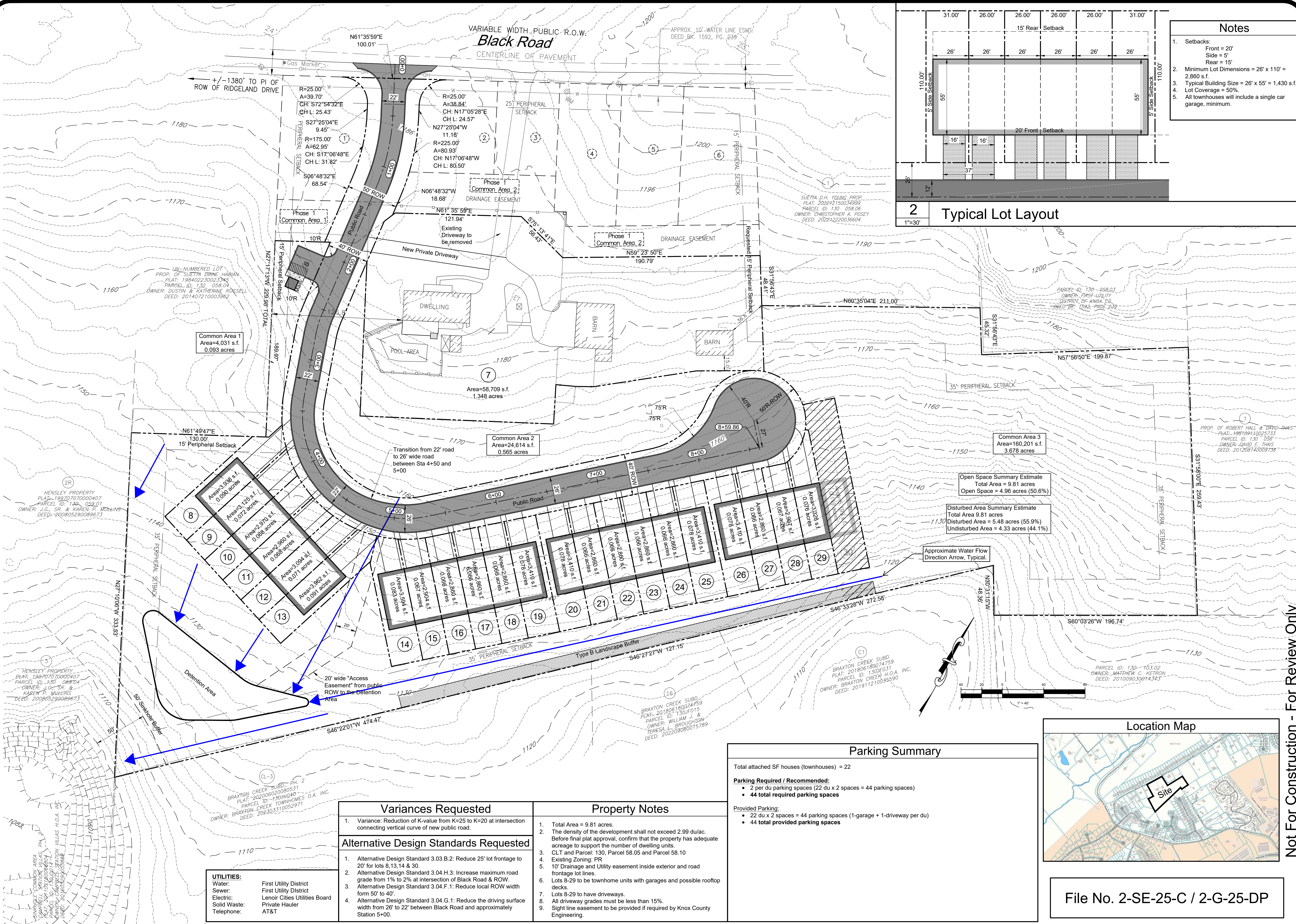


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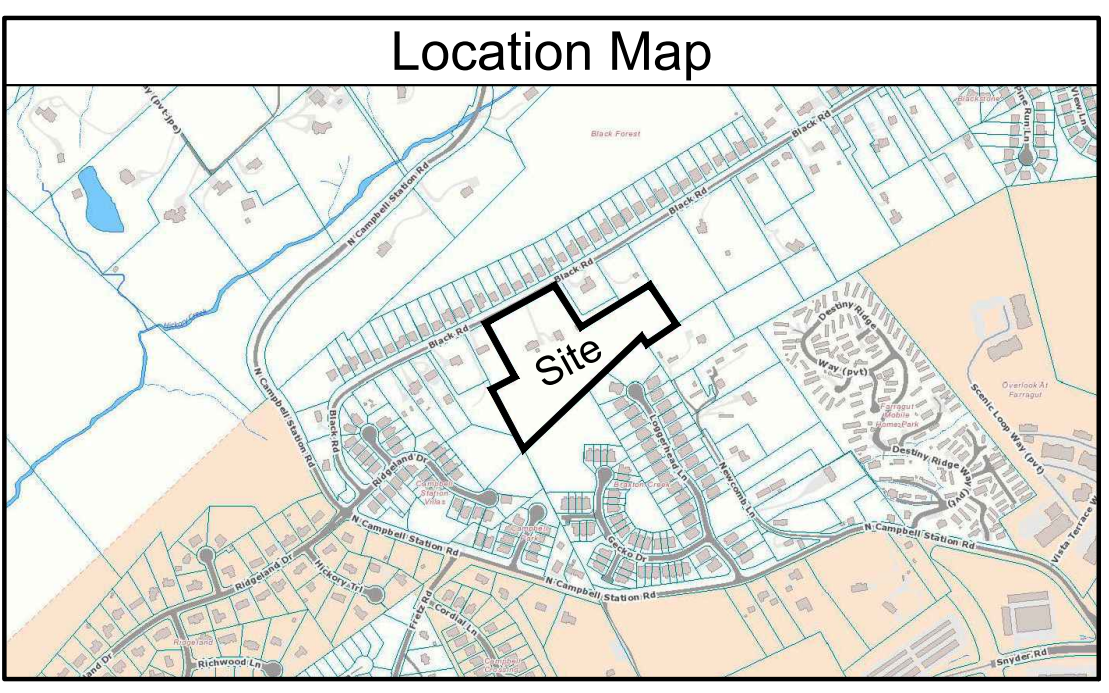
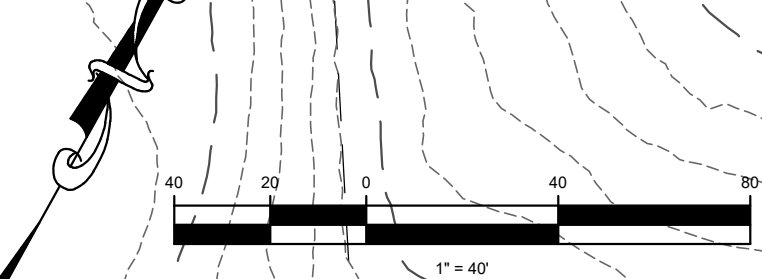


- ### Notes
1. Setbacks:  
Front = 20'  
Side = 5'  
Rear = 15'
  2. Minimum Lot Dimensions = 26' x 110' = 2,860 s.f.
  3. Typical Building Size = 26' x 55' = 1,430 s.f.
  4. Lot Coverage = 50%.
  5. All townhouses will include a single car garage, minimum.



**Open Space Summary Estimate**  
Total Area = 9.81 acres  
Open Space = 4.96 acres (50.6%)

**Disturbed Area Summary Estimate**  
Total Area 9.81 acres  
Disturbed Area = 5.48 acres (55.9%)  
Undisturbed Area = 4.33 acres (44.1%)



### Parking Summary

Total attached SF houses (townhouses) = 22

**Parking Required / Recommended:**

- 2 per du parking spaces (22 du x 2 spaces = 44 parking spaces)
- 44 total required parking spaces

**Provided Parking:**

- 22 du x 2 spaces = 44 parking spaces (1-garage + 1-driveway per du)
- 44 total provided parking spaces

Variations Requested	Property Notes
<ol style="list-style-type: none"> <li>1. Variance: Reduction of K-value from K=25 to K=20 at intersection connecting vertical curve of new public road.</li> </ol>	<ol style="list-style-type: none"> <li>1. Total Area = 9.81 acres.</li> <li>2. The density of the development shall not exceed 2.99 du/ac. Before final plat approval, confirm that the property has adequate acreage to support the number of dwelling units.</li> <li>3. CLT and Parcel 130, Parcel 58.05 and Parcel 58.10</li> <li>4. Existing Zoning: PR</li> <li>5. 10' Drainage and Utility easement inside exterior and road frontage lot lines.</li> <li>6. Lots 8-29 to be townhome units with garages and possible rooftop decks.</li> <li>7. Lots 8-29 to have driveways.</li> <li>8. All driveway grades must be less than 15%.</li> <li>9. Sight line easement to be provided if required by Knox County Engineering.</li> </ol>
Alternative Design Standards Requested	
<ol style="list-style-type: none"> <li>1. Alternative Design Standard 3.03.B.2: Reduce 25' lot frontage to 20' for lots 8, 13, 14 &amp; 30.</li> <li>2. Alternative Design Standard 3.04.H.3: Increase maximum road grade from 1% to 2% at intersection of Black Road &amp; ROW.</li> <li>3. Alternative Design Standard 3.04.F.1: Reduce local ROW width form 50' to 40'.</li> <li>4. Alternative Design Standard 3.04.G.1: Reduce the driving surface width from 26' to 22' between Black Road and approximately Station 5+00.</li> </ol>	

**UTILITIES:**  
Water: First Utility District  
Sewer: First Utility District  
Electric: Lenoir Cities Utilities Board  
Solid Waste: Private Hauler  
Telephone: AT&T

Not For Construction - For Review Only

No.	Date	Revision
1	12-05-2024	Knox Planning Comments
2	12-04-2024	Knox Planning Comments
3	12-30-2024	Knox Planning Comments
4	1-20-2025	Knox Planning Comments
5	1-23-2025	Knox Planning Comments



Drawn By	
Checked	
Approved	
Job No.	2024112
Scale	1"=40'
Date	11-25-2024

1 Concept Plan

Rev 1.23.2025

C101  
Sheet No.

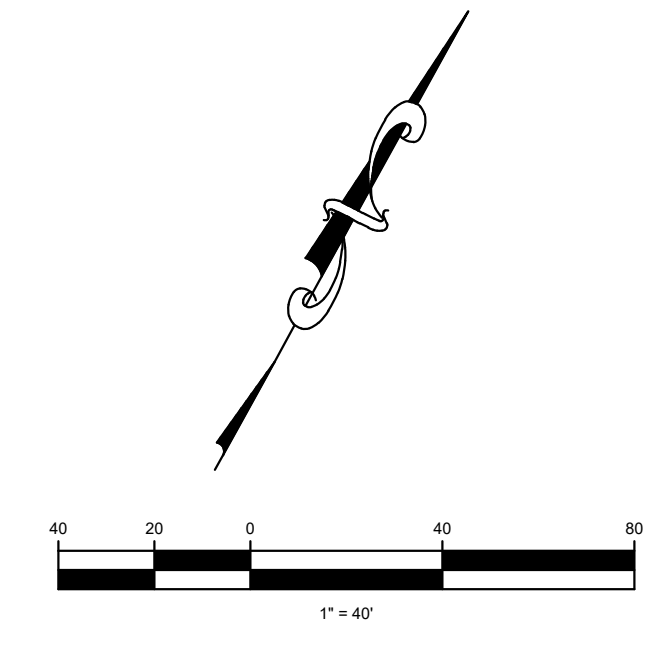
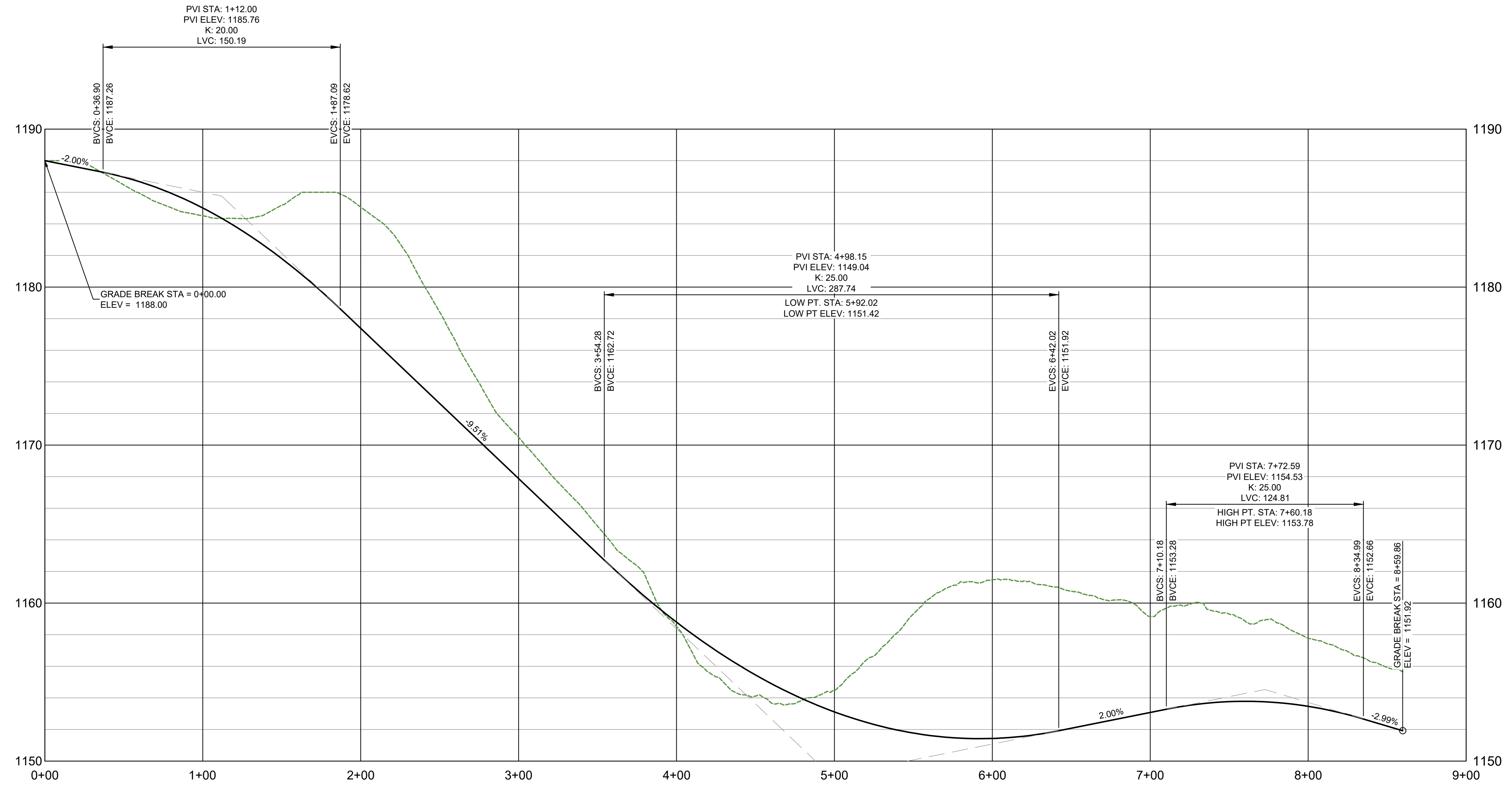
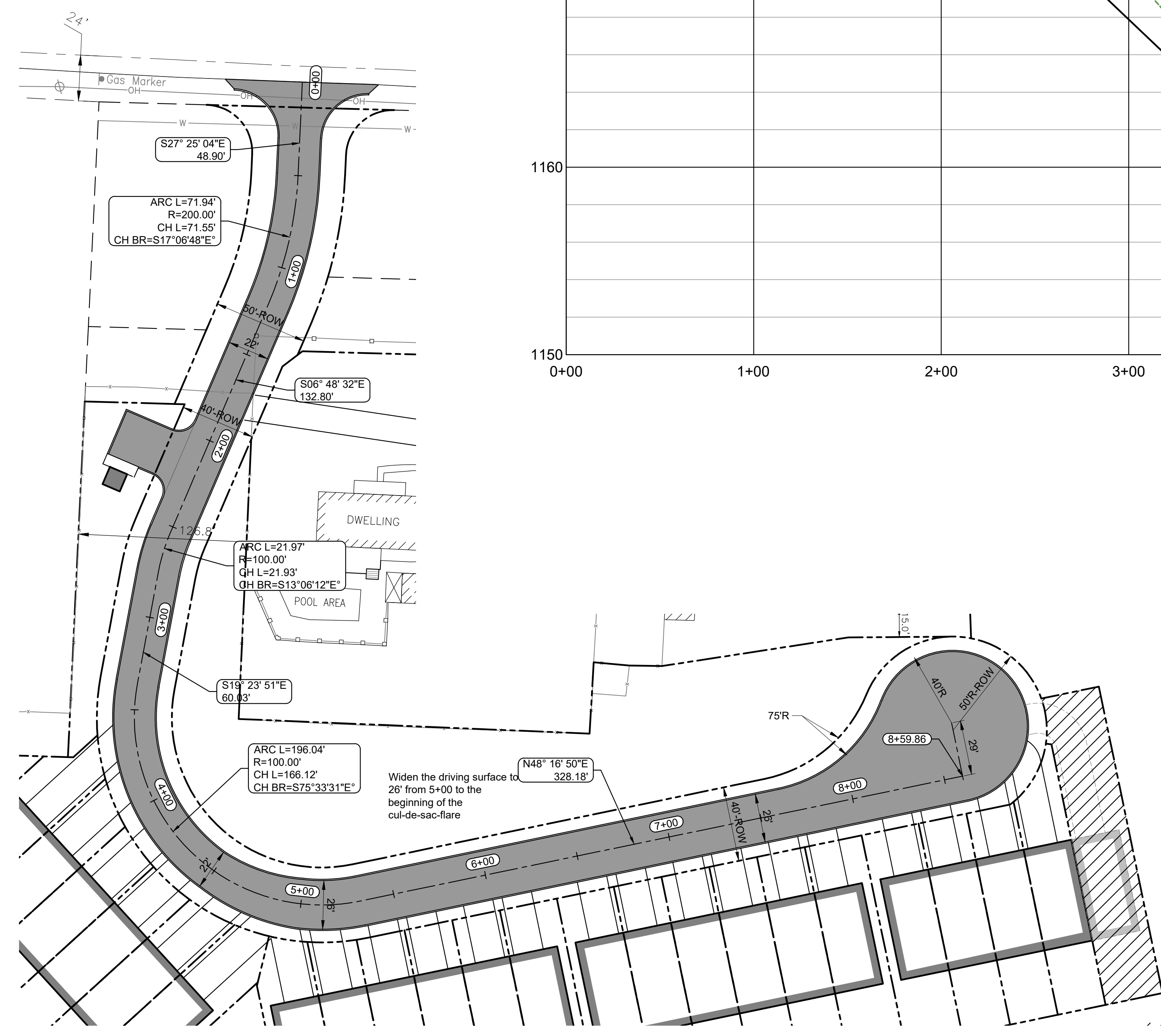
Project: Black Ridge Pointe Unit Two  
BBK Development, LLC  
Black Road  
Knox County, Tennessee



Drawing Description: Concept Site Plan

310 SIMMONS RD., SUITE K-KNOXVILLE, TENNESSEE 37922  
PH. 865-671-2281

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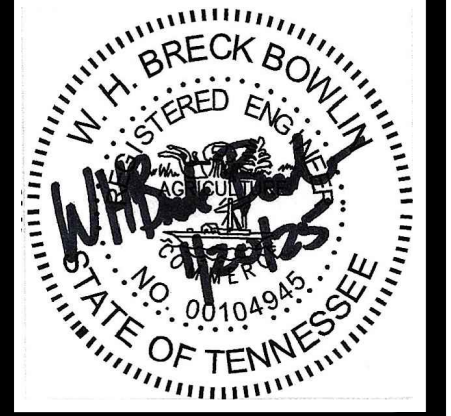
File No. 2-SE-25-C / 2-G-25-DP

1  
1"=40'

Concept Plan

Not For Construction - For Review Only

No.	Date	Revision
2	12-04-2024	Knox Planning Comments
3	12-30-2024	Knox Planning Comments
3	1-20-2025	Knox Planning Comments



Drawn By	
Checked	
Approved	
Job No.	2024112
Scale	1"=40'
Date	11-25-2024

C102  
Sheet No.

Drawing Description:  
Concept  
Road Plan and Profile

LAND DEVELOPMENT SOLUTIONS

310 SIMMONS RD., SUITE K-KNOXVILLE, TENNESSEE 37922  
PH. 865-671-2281

Project:  
Black Ridge Pointe  
Unit Two  
BBK Development, LLC  
Black Road  
Knox County, Tennessee

**BLACK RIDGE POINTE (UNIT-2) - DESIGN INTENT - TOWNHOUSE FRONT ELEVATION EXAMPLE RENDERINGS**

**FILE: 12-SD-24-C / 12-E-24-DP**



The point of this conceptual very limited consideration of potential roof top access is to satisfy your request to see architectural intent on vertical construction that has not started the design process. If rooftop access is provided, our intent would be to limit any roof-line modifications visible from the front as to not impact the appearance of the front elevation.

